



Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501

September 22, 2022

Addendum 1  
ITB #22-53-311675  
Curry Ballast Production

**Addendum number 1 has been issued for questions and closing date change.**

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## **New closing date – October 6, 2022**

### **Questions:**

1. In the project description and scope of work section 1.0 states a conceptual drawing of the development plan is attached for guidance purposes. Can you please provide that drawing?

**Yes. Drawing has been included at the end of this addendum and also attached on the ARRC webpage under this solicitation.**

2. The ARRC Load Manual does not provide maximum width, height, and weight tolerances for getting equipment to Curry. Can you please advise?

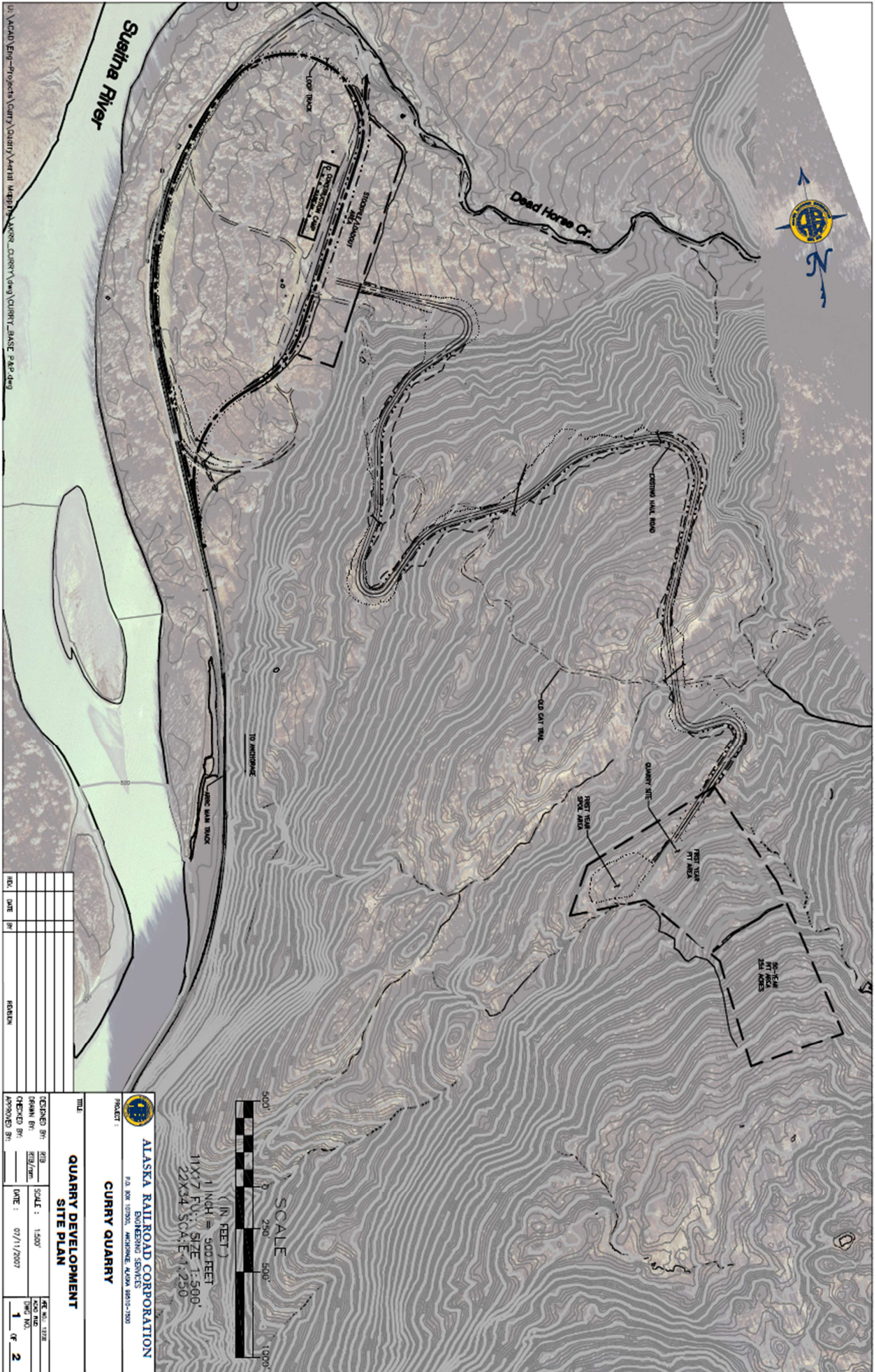
**Note that max width at the given height is above top of rail, ie loaded dimension. We usually assume a 3'9" ATR flat car deck height. See "Anchorage – Fairbanks Clearance Reference" provided, also attached on the ARRC webpage under this solicitation.**

**Typical flat car weight capacity is ~160k to 180k lb.**

All other terms and conditions remain unchanged.  
If there are any questions regarding this addendum please let me know.

Thank you,

*Rob Walker*  
Contract Administrator  
Alaska Railroad Corporation



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REV.	DATE	BY	REVISION

**ALASKA RAIL ROAD CORPORATION**  
 P.O. BOX 107000, ANCHORAGE, ALASKA 99510-7000

**ENGINEERING SERVICES**

**PROJECT:** CURRY QUARRY

**TITLE:** QUARRY DEVELOPMENT SITE PLAN

DESIGNED BY:	WJB	SCALE:	1:500'	DATE:	07/11/2007	REV. NO.:	1
DRAWN BY:	WJB/WJB					OF NO.:	2
CHECKED BY:							
APPROVED BY:							

Alaska Railroad Anchorage to Fairbanks Clearance Chart

4/5/2018

FOR REFERENCE ONLY; NOT FOR DETERMINING FINAL CLEARANCE.  
 PROVIDE LOADED DIMENSIONS TO CLEARANCE COORDINATOR FOR OFFICIAL CLEARANCE.

Height ATR	Maximum Allowable Width with:		
	No Restrictions	10 MPH Restriction	4 MPH Restriction
0'-0"	8'-8"	9'-2"	9'-8"
0'-6"	10'-3"	10'-9"	11'-3"
1'-0"	10'-11"	11'-5"	11'-11"
2'-0"	12'-2"	12'-8"	13'-2"
3'-0"	13'-5"	13'-11"	14'-4"
3'-6"	13'-7"	14'-1"	14'-7"
17'-0"	13'-7"	14'-1"	14'-7"
17'-3"	13'-4"	14'-0"	14'-7"
17'-6"	13'-0"	13'-6"	14'-3"
17'-9"	12'-8"	13'-2"	13'-8"
18'-0"	12'-3"	12'-9"	13'-3"
18'-3"	11'-11"	12'-6"	13'-2"
18'-6"	11'-6"	12'-2"	12'-9"
18'-9"	11'-2"	11'-10"	12'-5"
19'-0"	10'-10"	11'-4"	11'-10"
19'-3"	10'-5"	11'-1"	11'-8"
19'-6"	10'-0"	10'-8"	11'-4"
19'-9"	9'-6"	10'-2"	10'-11"
20'-0"	9'-0"	9'-9"	10'-5"
20'-3"	8'-7"	9'-3"	9'-11"
20'-6"	8'-0"	8'-9"	9'-6"
20'-9"	7'-5"	8'-0"	8'-10"
21'-0"	0"	7'-10"	8'-7"
21'-4"	0"	0"	7'-10"

Loads exceeding 13'-11" wide must coordinate with passing trains.

----- H-W detectors at 19'-6" ATR & 13'-6" Wide

ATR = Above Top of Rail