



## ALASKA RAILROAD CORPORATION

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December 9, 2025

### REQUEST FOR INTEREST (RFI)

25-25-RFI

### CYLINDRICAL ENERGY ABSORBERS FOR MARINE FENDER SYSTEM

#### GENERAL INFORMATION

The Alaska Railroad Corporation (ARRC) is a public corporation and instrumentality of the State of Alaska formed pursuant to AS 42.40 and is the last full service (passengers and freight) railroad in the United States, with a route that runs from Seward and Whittier at tidewater to the interior of Alaska just beyond Fairbanks. The corporate headquarters are in Anchorage. ARRC averages approximately 680 employees (year-round and seasonal), with employment reaching 750 in the summer season.

ARRC is a public corporation of the State of Alaska. More information about ARRC may be found at [www.alaskarailroad.com](http://www.alaskarailroad.com).

#### BACKGROUND

The Alaska Railroad Corporation has the need for procuring Build America, Buy America Act (BABAA) compliant energy absorbers for marine infrastructure expansion.

#### SCOPE OF WORK / REQUIRED MATERIAL

Energy absorber size and quantity:

1,200 x 600 (mm) cylindrical energy absorbers

12 ea at 4-feet long.

Test reports on verification testing shall include serial number and description of test units, location, and date of the test, printed name of the test supervisor, printed name and handwritten signature of the quality control manager, and date signed; table and graph of reaction and energy versus deflection; and signed third party certification of all test equipment (to be accurate within 1% +/-) dated no more than one year prior to test date.

Energy absorber performance requirements:

Energy Absorption: 145 kip-ft minimum per 4-feet unit (at manufacturing & performance tolerance)

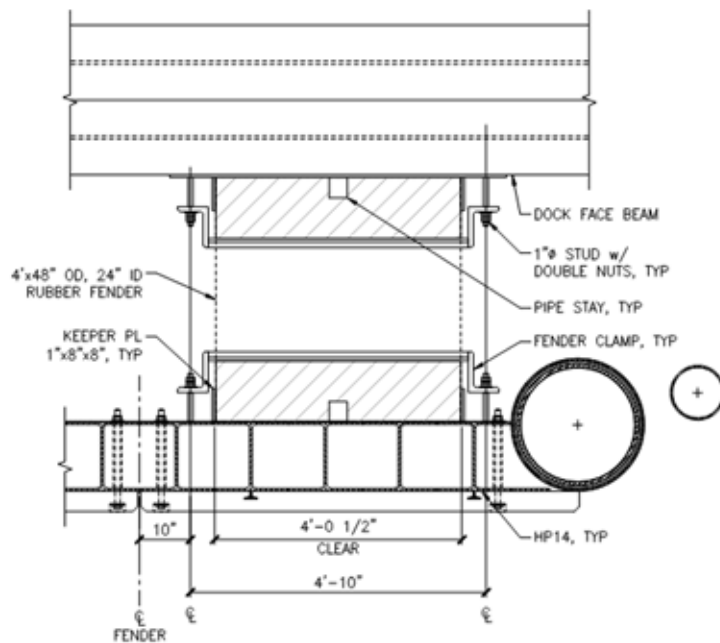
Deflection: 50% of OD

All rubber energy absorbers shall be pre-conditioned by compressing them to the appropriate rated deflection at least three (3) times. Compression shall occur uniformly and symmetrically as to cause the energy unit to deform as intended by the Manufacturer. Verification testing shall be performed on 10% of units rounded up to the nearest whole number.

During the verification test, compression shall be applied toward the top face of the fender, compression speed shall follow current PIANC fender performing testing guidelines and shall be recorded during testing.

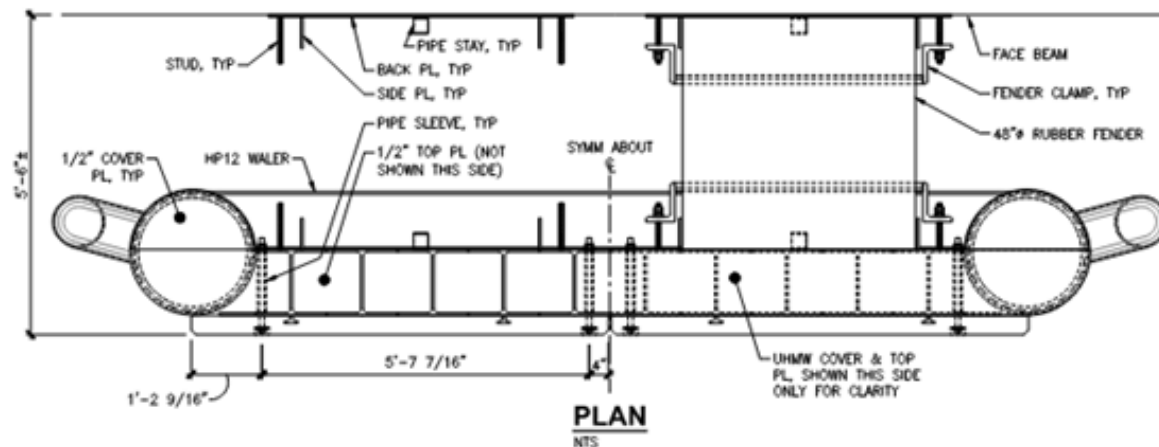
1. The fender is to be cycled for three times up to the designed deflection.
2. Let the fender stand for at least one hour.
3. A fourth deflection cycle shall then be performed using constant or decreasing velocity compression. The fourth cycle shall determine the fender performance.
4. The room temperature at the time of the test shall be recorded.
5. Correction factors for the fender performance shall be determined per PIANC for the temperature recorded at the time of the test.
6. All fenders shall achieve a performance with  $\pm 10\%$  of the stated nominal design performance.

Contractor shall notify the engineer at least two weeks prior to verification testing. Verification tests shall be witnessed by an accredited 3rd party testing agency. Certificates shall be provided before the fenders arrive on the jobsite confirming that the tests were performed according to the current PIANC standards. The certificates shall also confirm that the results of the tests meet the energy and reaction requirements as specified above. If any unit fails the verification test it shall be rejected. If any unit fails, all units of the same size shall be verification tested at no additional expense to the owner.



**FENDER ATTACHMENT PLAN**

NTS



**PLAN**

NTS

**Important Notice:** The Alaska Railroad does not intend to award a contract from this RFI, nor will it be financially responsible for any costs associated with the preparation of any response for the requested information. This RFI is issued for the sole purpose of obtaining information as described in this notice.

## **RESPONSE INFORMATION**

Your response must include details on your company's experience, qualifications, services offered, and why your product will work for the ARRC.

Interested firms must indicate their interest by submitting an electronic response (pdf format is preferable) with the following information:

- Firm Name, Address, Contact Name, Phone number, Email address
- Address how your firm meets the scope requirements.

This Request for Interest does not constitute a formal solicitation. ARRC is not responsible for any costs associated with the preparation of responses. The issuance of the Request for Interest provides no guarantee that the ARRC will proceed with a formal solicitation.

Please submit an electronic response no later than 3:00 PM Alaska Time, December 17 to the attention of:

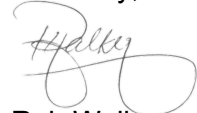
Alaska Railroad Corporation  
Rob Walker  
Logistics Manager  
Email: [WalkerR@akrr.com](mailto:WalkerR@akrr.com)

On the subject line of the email please provide: 25-25-RFI ....

Questions can be addressed by email to the individual listed above.

Please note ARRC does not accept responsibility for failed e-mailed response deliveries. It is the responsibility of the interested party to follow up with the individual listed above to ensure your response was received prior to the deadline specified above.

Sincerely,



Rob Walker  
Logistics Manager  
Alaska Railroad, Corporation